



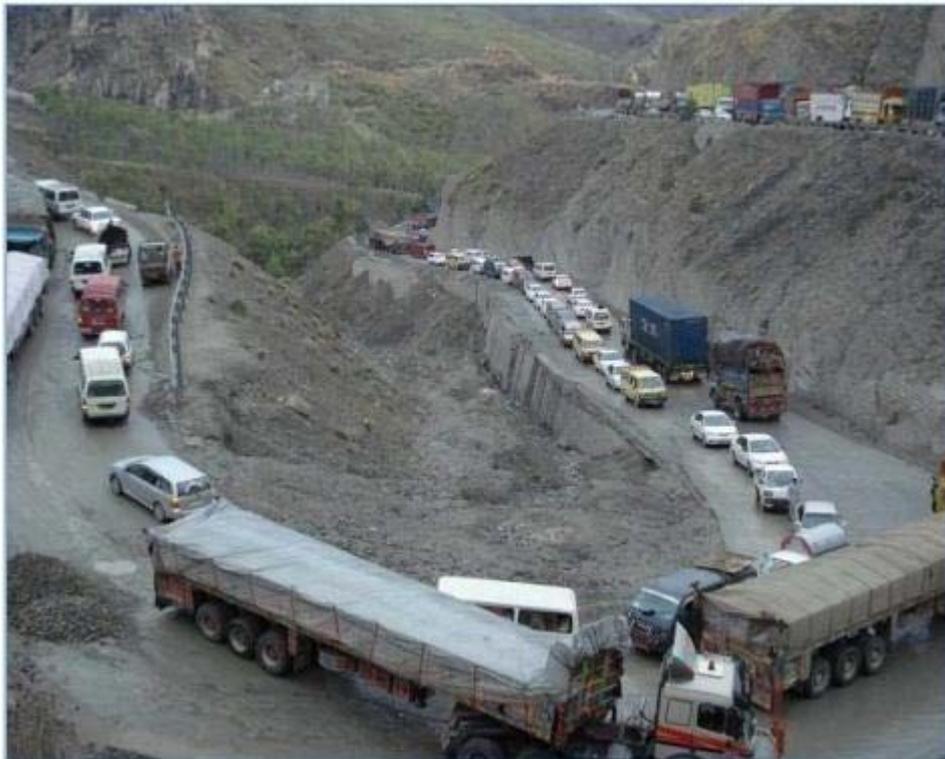
USAID | **PAKISTAN**
FROM THE AMERICAN PEOPLE

CONSTRUCTION MONITORING & EVALUATION PROGRAM

**STRENGTHENING & IMPROVEMENT OF PESHAWAR – TORKHAM
ROAD (N-5), KHYBER AGENCY, FATA**

CONTRACT NO. SOL-391-12-000038

QUARTERLY PROGRESS REPORT # 02



JANUARY-MARCH 2013

M&E Consultants



AL-KASIB GROUP OF ENGINEERING SERVICES

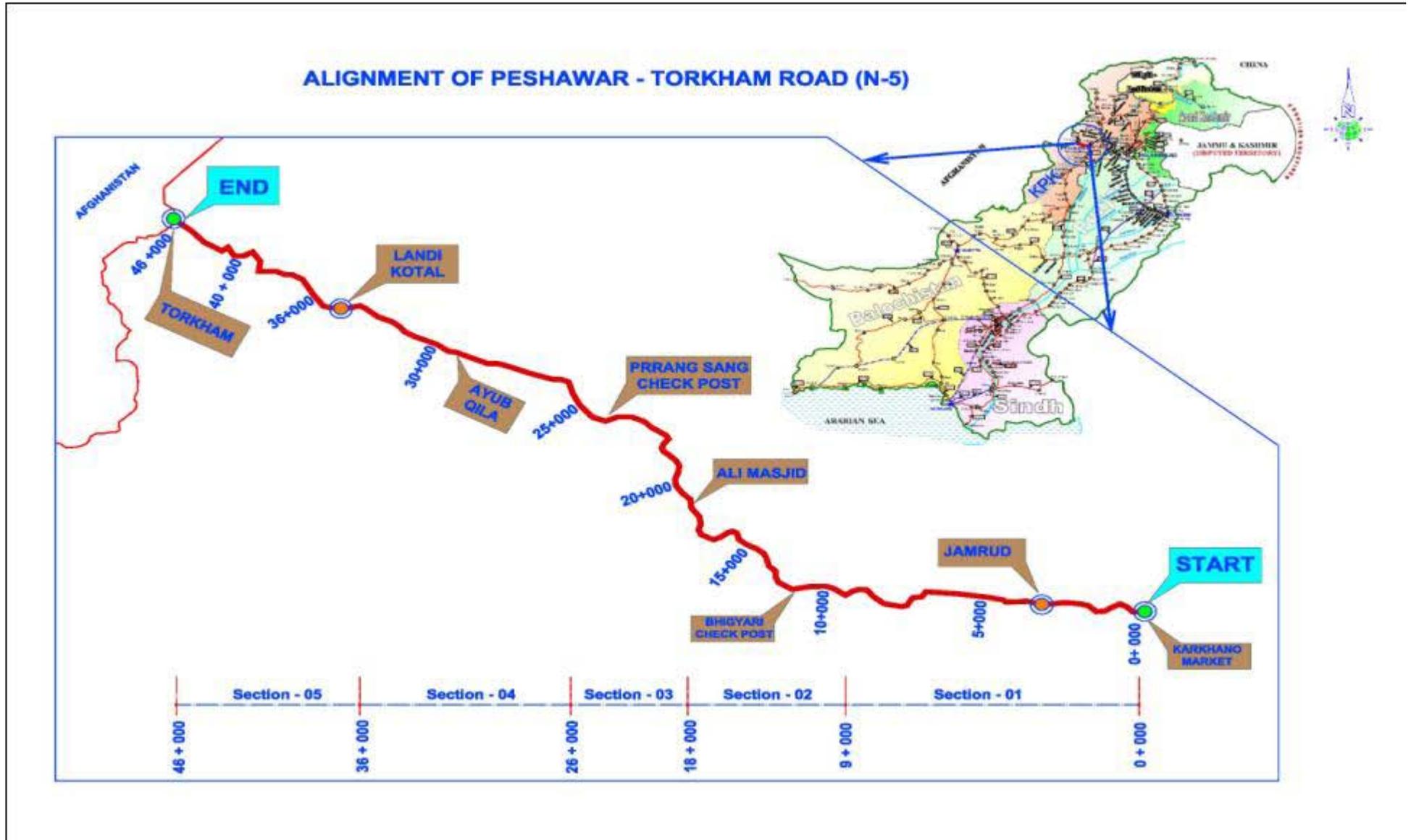
H # 22, Street 1, Phase 7, Hayatabad Peshawar, Khyber Pakhtunkhwa Pakistan
Tel: 0092 91 5863291-92-93; Fax: 0092 91 5863294
Email: ages@ages.com.pk



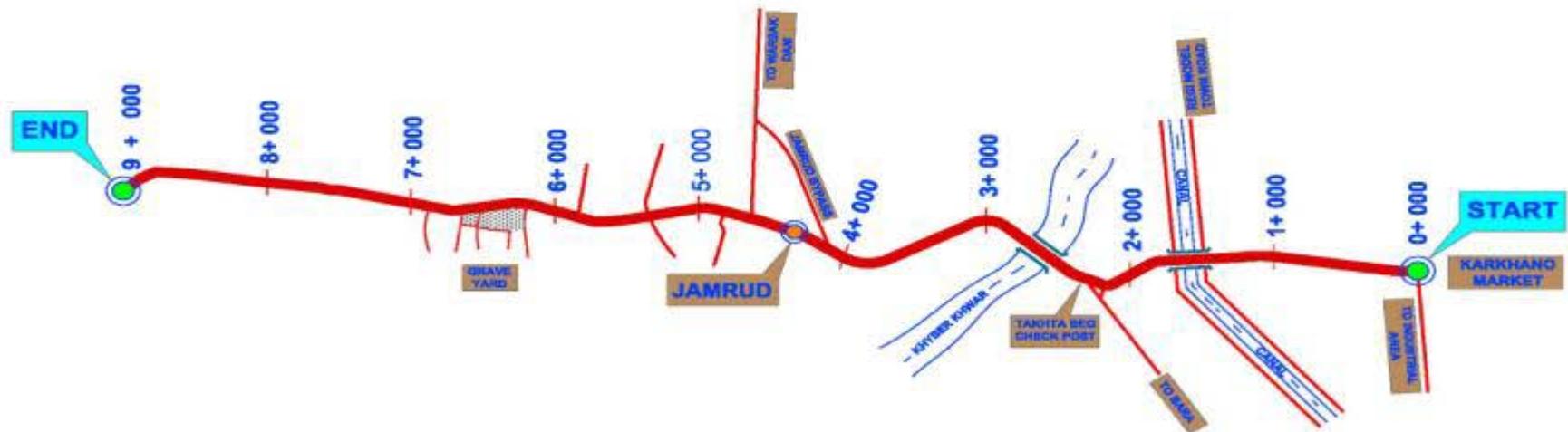
TABLE OF CONTENTS

LOCATION MAP	1
EXECUTIVE SUMMARY	3
PROJECT IMPEDIMENTS	4
1 INTRODUCTION	5
1.1 PROJECT BACKGROUND:	6
1.2 SCOPE OF WORK.....	6
1.3 LINE SKETCH OF ALIGNMENT.....	7
1.4 TYPICAL CROSS SECTION OF ROAD	8
2 MONITORING & EVALUATION SERVICES	9
2.1 MOBILIZATION OF M&E STAFF	10
2.2 ORGANIZATION CHART FOR CMEP OFFICE, PESHAWAR	11
2.3 ORGANIZATION CHART FOR ROAD COMPONENT OF CMEP PROJECT	12
2.4 M&E CONSULTANTS ACTIVITIES DURING THE QUARTER	13
3 CIVIL WORKS	14
3.1 GENERAL CONTRACT DATA	15
3.2 PHYSICAL PROGRESS STATUS	16
3.3 CUMULATIVE FINANCIAL STATUS.....	17
3.4 QUARTERLY PROGRESS BY BILL OF QUANTITIES	18
3.5 CULVERTS PHYSICAL PROGRESS STATUS	19
3.6 DETAILED INFORMATION OF LABORATORY TEST REPORT	20
4 ENVIRONMENTAL COMPLIANCE MONITORING	22
4.1 INTRODUCTION:.....	23
4.2 ENVIRONMENT COMPLIANCE.....	24
4.3 PROGRESS DURING THE QUARTER # 02 (JAN – MARCH 2013)	25
5 SECURITY REPORT	26
6 APPENDICES	29
6.1 ELECTRONIC COMMUNICATION/CORRESPONDENCE WITH USAID	30
6.2 RECORD OF COORDINATION MEETING	31
7 PROJECT PHOTOGRAPHS	32

LOCATION MAP



**ALIGNMENT OF PESHAWAR - TORKHAM ROAD (N-5)
 (SECTION - I)**



EXECUTIVE SUMMARY

Peshawar – Torkham road is part of the overall Contract that includes Construction Monitoring and Evaluation Services of 114 schools and 100-200 KM roads in Khyber Pakhtunkhwa Province. This road connects Pakistan with Afghanistan at Torkham border and plays an important role in the economic activities as well as in providing timely logistic support to the security agencies deployed in Khyber Agency. The project is funded with USAID grant and implemented by FATA Secretariat through FWO (Frontier Works Organization) as EPC (Engineer, Procure, and Construct) Contractor. FATA/FWO has retained the services of M/S NESPAK as the Project Consultants to design and supervise the construction work, while USAID has engaged M/S AGES as M&E Consultants to monitor and report on quality as well as progress of the project. The project is anticipated to be completed by December 31, 2014.

To expedite construction activities, the 46 KM Peshawar – Torkham road has been divided in five sections. Work on section – I (9 KM) was initiated by FWO on October 15, 2012. During first quarter (Oct-Dec, 2012), major activities included: office establishment, mobilization of staff/machinery, surveying, identification of borrow material sources, earthwork and dismantling of existing structures; hence the progress of construction work was relatively slow (less than 1%). During 2nd/last quarter (Jan-March, 2013), FWO has made significant progress (6.68%) despite several extended periods of heavy rainfall and security related incidents. It is also worth-mentioning that the on-site interaction and cooperation between FWO/NESPAK and M&E staff has been improved significantly. However, with a lapse of about six months (21% of total time), timely completion of the project seems to be very challenging. Major activities and accomplishments made during the last quarter are summarized below:

- Traffic Diversion extended for further 3.26 KM. Total Diversion is 5.26 KM
- Three sources for sub-base/base course material were identified
- Quality Control tests were conducted both by FWO as well as M&E Consultants
- Earthwork: 186.65 % (exceeds PC-I quantity for earthwork)
- Sub Base: 33.93 %
- Culverts: 44.30 %
- Cross sectional survey for section-II (KM: 9 – 18) was carried out
- FWO/NESPAK enhanced their manpower and machinery / other resources
- M&E Consultants regularly visited the site and submitted progress report each month.

In order to complete the project within the stipulated timeline, the Contractor FWO / NESPAK must expedite their work on the planning, designing and construction of all sections of the project. The Contractor is also advised to expedite work on the following core elements:

- Assessment of Existing Bridges for their structural integrity, traffic capacity, approaches geometry, protection works etc.
- Designing and preparing Drawings of new bridges on the basis of site hydrology and Geotechnical investigation.
- Horizontal / Vertical clearance issue at Bab – e – Khyber monument.
- Mobilization, Installation, Testing and Commissioning of Asphalt Plant.

PROJECT IMPEDIMENTS

For effective monitoring of work in progress, the M&E Consultants are still waiting for the baseline schedule from FWO/NESPAK side. Similarly, for quality control, FWO/NESPAK has been unable to submit their Quality Control (QC) Plan. In absence of the said two vital documents, control over progress as well as quality remains very challenging.

In order to complete the project expeditiously, the understanding was that FWO will plan and carry-out construction activities simultaneously along different sections of the Road. However, by end of the 2nd quarter, FWO could hardly initiate work on section-II in addition to section-I. It is also to mention that even for section-II, the project documents (Design/PC-I, drawings, traffic diversion plan, etc.) are not finalized yet. It is recommended that FWO/NESPAK should review/revise their project management plan, enhance their resources, pay due diligence in selection & supervising of sub-contractors, and adopt multi tasking strategy in order to enhance their progress towards timely completion of the project. FWO is also requested to submit necessary documents for section-II (Km 9 to 18) to the M&E Consultants for their review and use.

The Contractor's traffic management plan through the construction zone (section – I) and Detour needs drastic improvement. Presently, both geometry as well as surface/riding quality of the detour is miserable, which is causing great inconvenience and safety concern to the road users. This issue has been highlighted during in-person meetings as well as technical reports; however, compliance by FWO is next to none.

Last but not the least, issues related to quality of the construction materials and workmanship should be properly addressed as these may also hinder the progress of work. It is suggested that FWO should adopt effective techniques for supervising/controlling the onsite construction activities, especially in the sub-contracted works.

INTRODUCTION

1.1 PROJECT BACKGROUND:

The Federally Administered Tribal Area (FATA) Secretariat of the Government of Pakistan (GoP) under the Quick Impact Projects (QIPs) in the Khyber Agency has inked an agreement with USAID for financial assistance in the form of a Grant for Strengthening and Improvement of 46 KM long existing two-lane, two-way carriageway from Peshawar to Torkham (N – 5). The Project will support the GoP in improving accessibility to the remotely located areas of Khyber agency and enhance logistic support to law enforcing agencies, besides assisting trade between Pakistan and Afghanistan. The Sponsoring agency for the Peshawar Torkham Road Project is FATA secretariat, headed by Additional Chief Secretary FATA. The Executing agency is Frontier Works Organization (FWO).

Table: 1

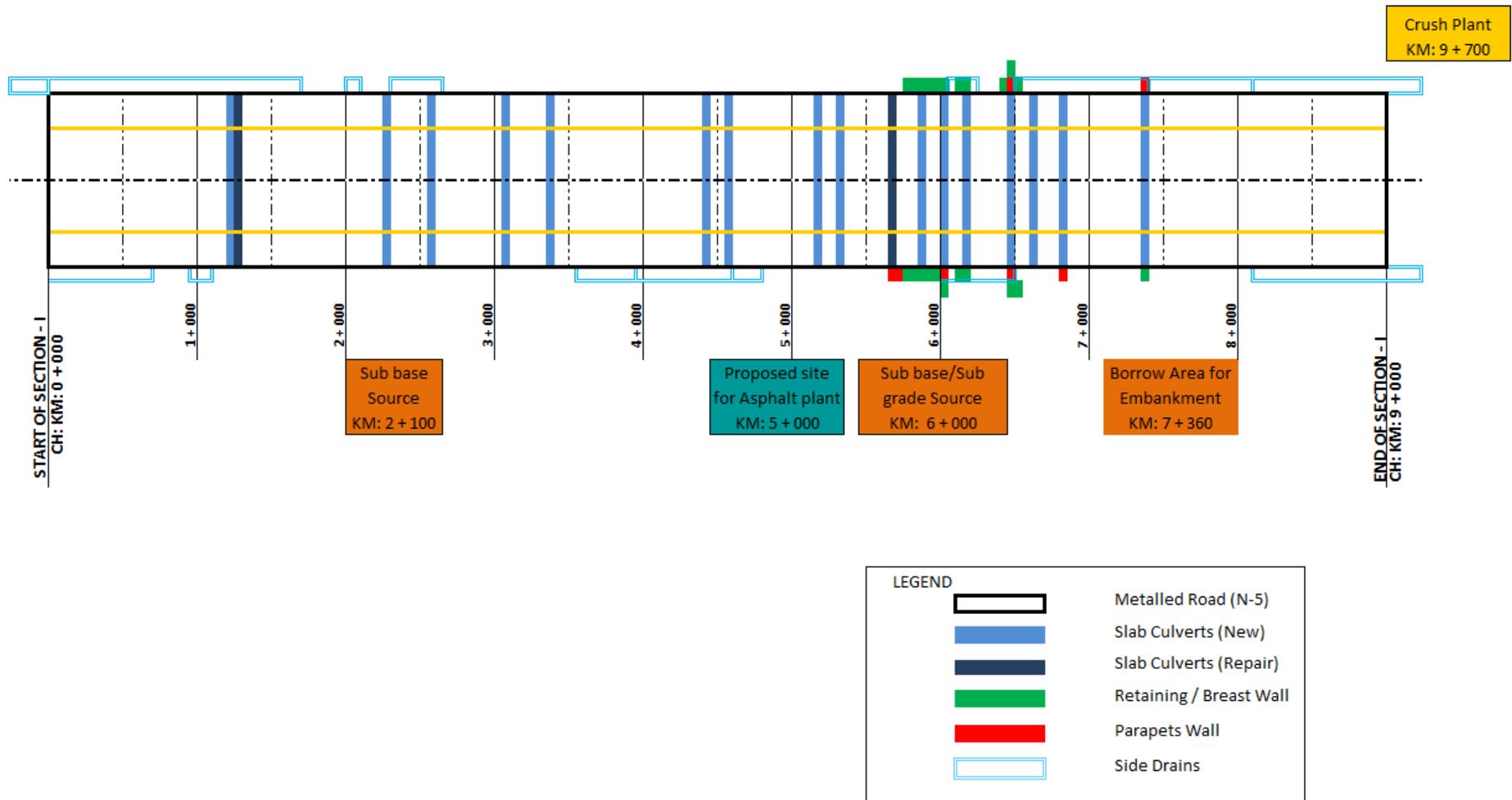
Civil Works Package Features					
Feature	Section – I	Section – II	Section – III	Section – IV	Section – V
Physical Limits	Peshawar to Torkham				
Kilometers	0+00 to 9+00	9+00 to 18+00	18+00 to 26+00	26+00 to 36+00	36 to 46
Black Top	Total 12.3 meter - 7.3 meter carriageway and 2.5 meter shoulder on either side				
Donor Agency	USAID				
Completion Period	807 Days				
Contract Forms	Conditions of Contract for EPC (Engineer, Procure, Construct)/Turnkey Projects (FIDIC Conditions of Contract – 1999)				

1.2 SCOPE OF WORK

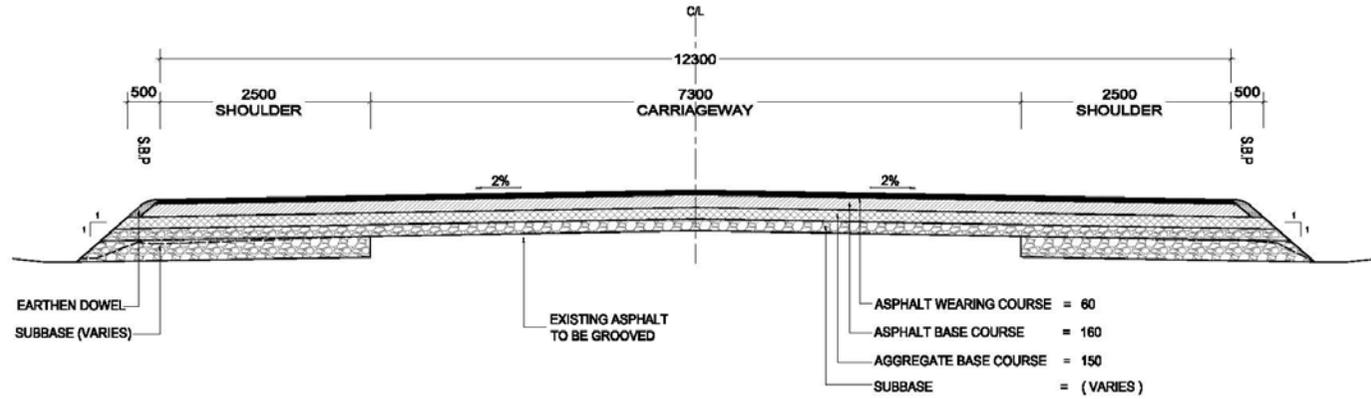
The project involves widening, strengthening and improvement of the existing two lane carriageway, including construction of new cross drainage structures, bridges and earth retaining structures. At a first stage, the FATA Secretariat has undertaken to contract out section – I of the project from KM: 0 +000 To KM: 9 + 000. Length of each package varies between 08 and 10 KM.

Being an EPC form of contract, FWO is fully responsible for design and construction of the project in conformity with the NHA's specifications and standard engineering practices. AGES Consultants has been awarded the Construction Monitoring and Evaluation Services including Quality Assurance and Environmental Monitoring of the project on behalf of the USAID Pakistan Mission.

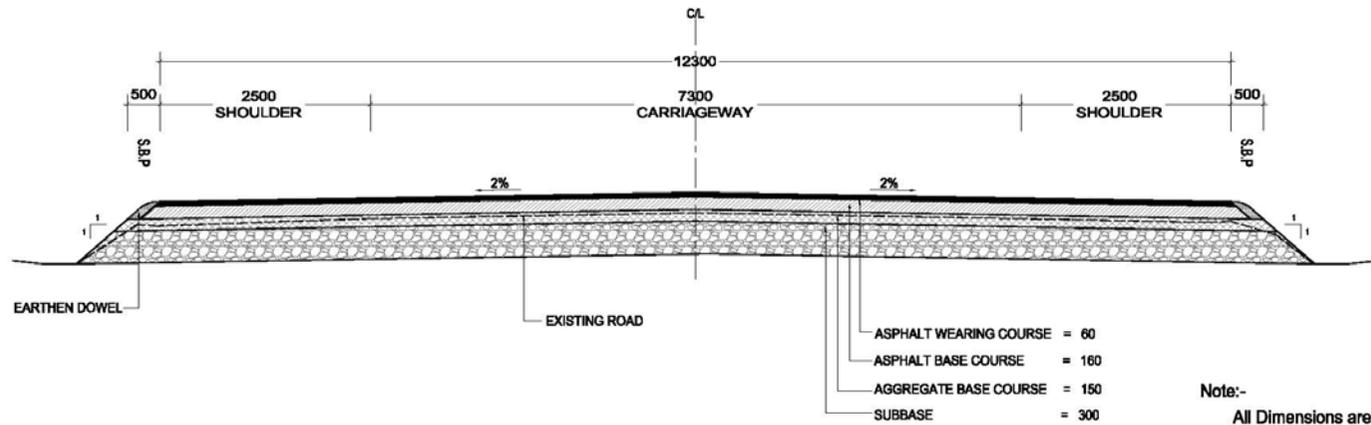
1.3 LINE SKETCH OF ALIGNMENT



1.4 TYPICAL CROSS SECTION OF ROAD



TYPICAL CROSS SECTION IN FILL



Note:-
 All Dimensions are in mm

TYPICAL CROSS SECTION IN CUT

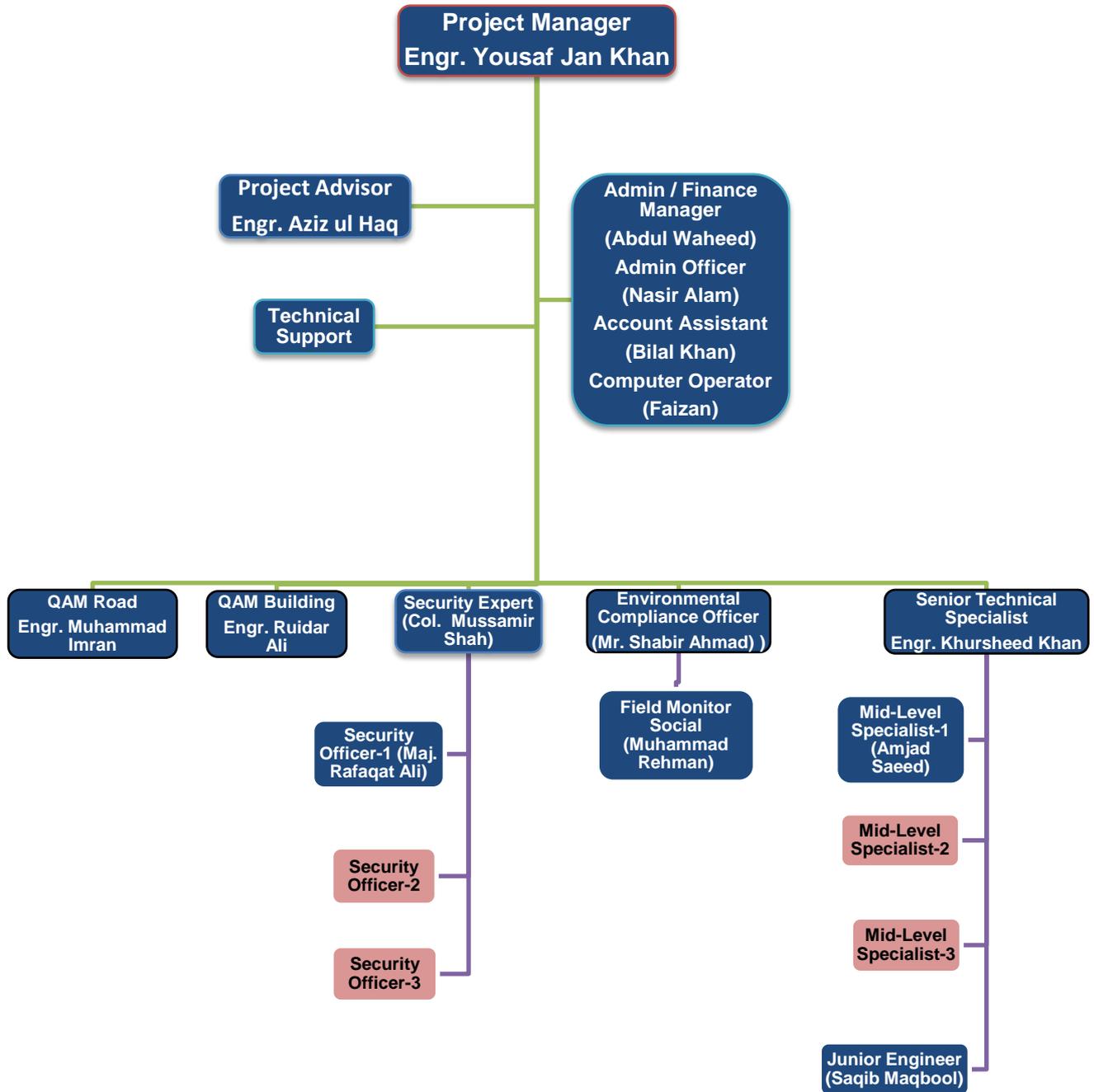
MONITORING & EVALUATION SERVICES

2.1 MOBILIZATION OF M&E STAFF

Following mobilization of the Project Manager and Quality Assurance Manager, other members of the M&E Team were mobilized as various activities of the project progressed. A few more field staff members will be mobilized once construction on other sections of the project is initiated. The existing M&E staff is listed below along with their designations and date of mobilization:

S. No.	Name	Designation	Date of Mobilization
1	Yousaf Jan Khan	Project Manager	01-10-2012
2	Muhammad Imran	Quality Assurance Manager	01-10-2012
3	Fakhr uz Zaman	M&E Specialist	12-12-2012
4	Muhammad Ilyas	Field Manager M&E	31-12-2012
5	Gul Zada	Material Engineer	24-12-2013
6	Muhammad Ibrar	Office Engineer	16-11-2012
7	Rasheed Khan	Field Monitor	15-01-2013
8	Inayatullah Khan	Quantity Surveyor	16-11-2012
9	Sohail Anjum	Senior Surveyor	01-10-2012
10	Naeem Jan	Senior Surveyor	20-12-2012
11	Muhammad Waqas	Survey Assistant	01-01-2013
12	Muhammad Ayaz	Survey Assistant	01-01-2013
13	Amjad Ali Khan	Senior Lab. Technician	28-01-2013
14	Khan Umar	Senior Lab. Technician	11-02-2013
15	Zeeshan Atta	Survey Assistant	05-01-2013
16	Shakeel Akbar	Laboratory Technician	30-01-2013
17	Noor Ali Jan	Laboratory Technician	31-01-2013
18	Baber Naeem	Asstt. Lab. Tech.	01-01-2013
19	Mujeeb Khan	Asstt. Lab. Tech.	22-01-2013
20	Asad Khan	CAD Operator	28-01-2013
21	Hamid Ali	Computer Operator	25-02-2013

2.2 ORGANIZATION CHART FOR CMEP OFFICE, PESHAWAR



LEGEND:

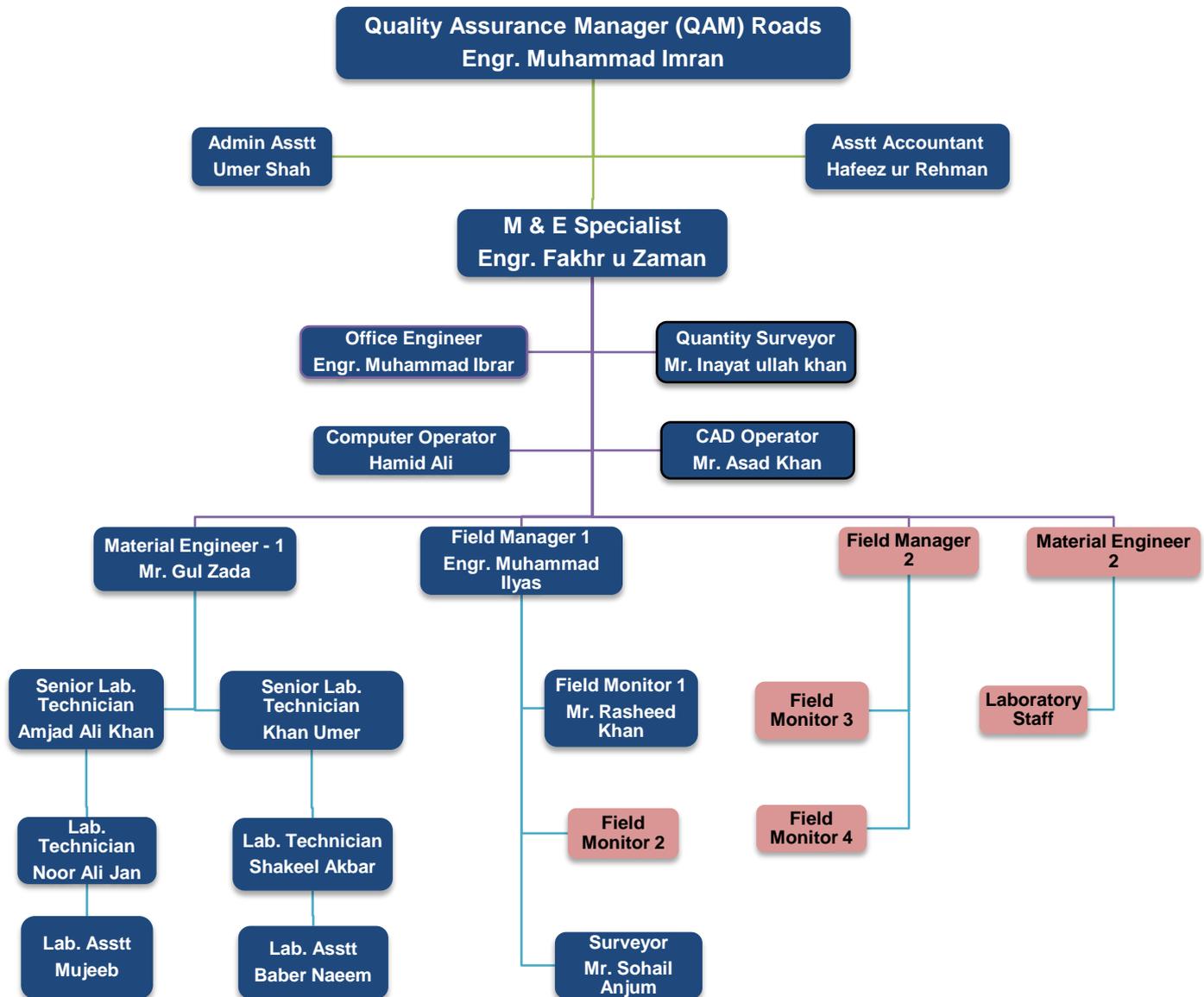


Mobilized



To be mobilized with expansion of work

2.3 ORGANIZATION CHART FOR ROAD COMPONENT OF CMEP PROJECT



LEGEND:



Mobilized



To be mobilized with expansion of work

2.4 M&E CONSULTANTS ACTIVITIES DURING THE QUARTER

During the reporting quarter, M&E Consultants have carried out the following activities:

- Conducted 02 Joint site visits with representatives of USAID and FWO / NESPAK.
- Conducted follow-up / coordination meetings with USAID & FWO / NESPAK reps.
- Monitoring / documentation of the construction activities on daily basis.
- Conducting fortnight site visits of M&E Consultant's senior management and preparation / submission of facts finding report to USAID.
- Maintaining close liaison with the Contractor's field staff and sharing of information pertaining to material quality and construction methodology.
- Conducting joint field density testing of different pavement layers / backfill material with FWO / NESPAK.
- Reviewed / Evaluated Contractor's Base line Schedule / Work Plan and submitted technical comments to USAID.
- Reviewed / Evaluated Contractor's Draft Quality Control Plan and submitted technical comments to USAID.
- M&E Consultants Material Testing Laboratory has been established for conducting necessary quality tests of Soil / aggregate, Concrete and Asphalt etc.
- Regular monitoring and reporting of issues related to detour along the construction zone; for example:
 - ❖ Traffic operating conditions with regard to detour geometry, surface condition, visibility and traffic safety / management.
 - ❖ Dust suppression activities, particularly during peak traffic hours.
 - ❖ Climatic / rainfall impact, etc.

During their onsite discussions with FWO/ NESPAK staff, the M&E Consultants provided suggestion for effective maintenance of Detours; including

- Regular grading of surface and patching of potholes/depressions
- Installing temporary pipe culverts for smooth flow of rain water across the Detour
- Installation of signs for traffic safety and operation, during day and night time
- Geometric improvements

CIVIL WORKS

3.1 GENERAL CONTRACT DATA

1.	Name of Project	Strengthening and Improvement of Peshawar Torkham Road (N-5) Khyber Agency FATA.
2.	Name of Package	Section – I (CH: KM: 0+000 To CH: KM: 9+000)
3.	Sponsoring Agency	FATA Secretariat, Peshawar
4.	Sponsoring Agency Representative	Mr. Roshan Mahsud, Project Director, PMU FATA
5.	Donor Agency	USAID PAKISTAN
6.	Donor’s Agency Representative	Engr. Farhat Banori, USAID/COR
7.	Executing Agency	Frontier Works Organization
8.	Executing Agency Representative	Lt. Colonel Khurram
9.	M&E Consultants	AGES Consultants (Pvt) Ltd.
10.	M&E Consultants Representative	Engr. Yousaf Jan Khan, Project Manager
11.	Project Cost (Section – I)	Rs. 937.939 Million
12.	Time for Completion	807 Days
13.	Mode of Construction Contract	EPC (Engineer, Procure and Construct) Contract
14.	Chronology	
	Signing of MoU (USAID–FATA–NHA)	Sep 18, 2012
	Signing of Contract (USAID – AGES)	Sep 30, 2012
	M&E Consultants Mobilization	Oct 01, 2012
	Approval of PC – 1	Nov 20, 2012
	Project Date of Commencement	Oct 15, 2012
	Project Date of Completion	Dec 31, 2014

3.2 PHYSICAL PROGRESS STATUS



LEGEND



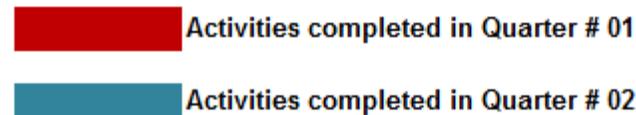
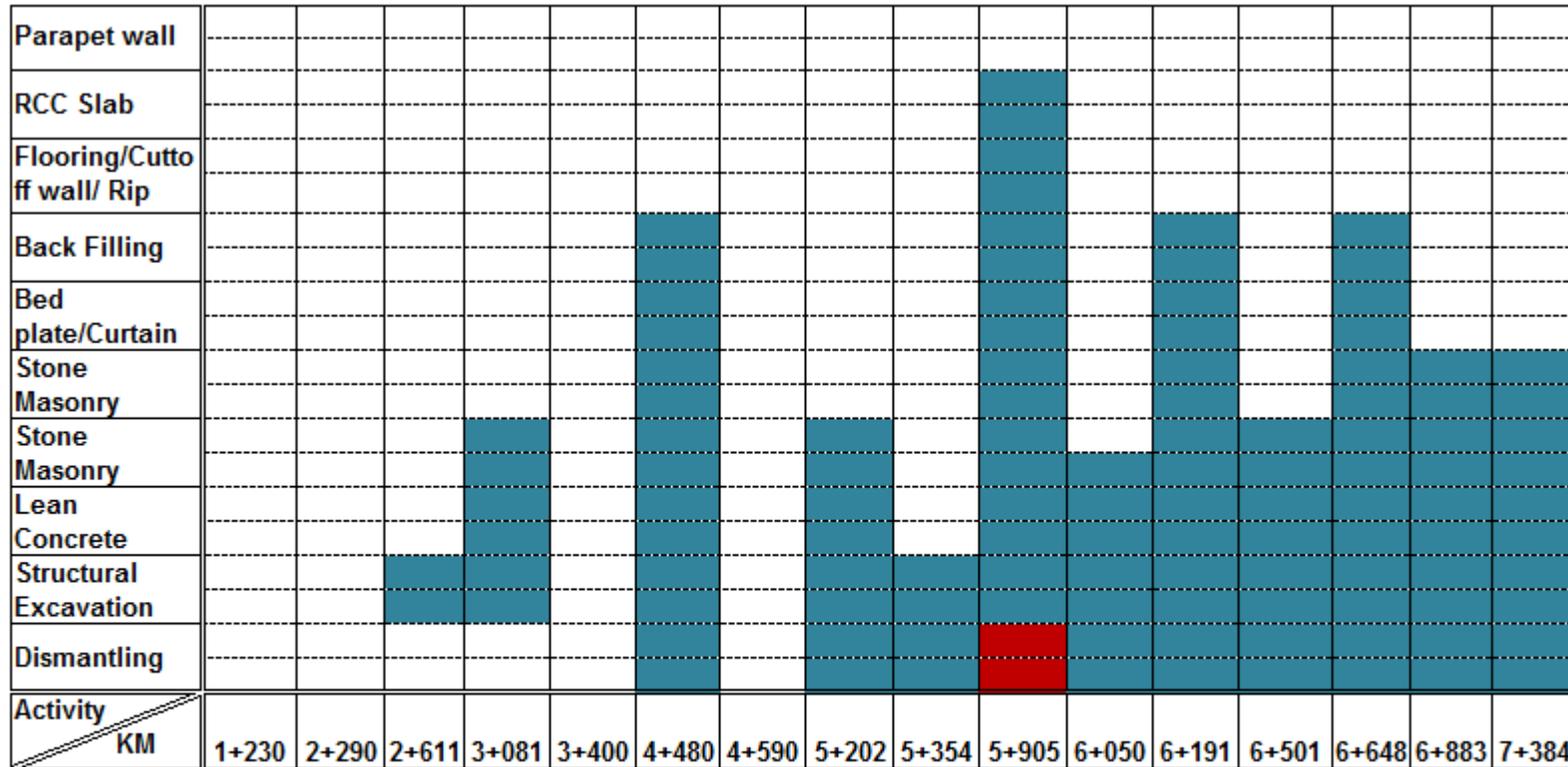
3.3 CUMULATIVE FINANCIAL STATUS

CONTRACT			QUARTER # 01		QUARTER # 02	
BILL NO	DESCRIPTION	AMOUNT (Rs.)	AMOUNT (Rs.)	PROGRESS %	AMOUNT (Rs.)	PROGRESS %
1	EARTH WORK	4,396,321.49	910,698.31	20.72	8,205,599.00	186.65
2	SUB BASE AND BASE COURSE	417,440,419.46	-	-	26,293,710.87	6.30
3	SURFACE COURSES AND PAVEMENT	148,248,125.37	-	-	664,719.75	0.45
4a	STRUCTURES (RETAINING WALL/BREAST WALL)	2,990,459.56	-	-	-	-
4b	STRUCTURES (CULVERTS)	34,156,831.05	-	-	15,129,971.31	44.30
5a	DRAINAGE & EROSION WORKS (ROAD SIDE DRAIN)	146,629,248.32	-	-	-	-
5b	ROAD PROTECTION WORKS	851,203.80	-	-	-	-
6	ANCILLARY WORKS	4,189,586.08	-	-	-	-
7	DIVERSION	9,000,000.00	377,500.00	4.19	1,055,000.00	11.72
8	RELOCATION OF UTILITIES	900,000.00	-	-	-	-
Sub Total - Construction Cost		768,802,195.13	1,288,198.31	0.17	51,349,000.92	6.68
INDIRECT COST	Contingencies @ 0.5% of Total Construction Cost	3,844,010.98	6,440.99	0.17	256,745.00	6.68
	EPC Turnkey Cost	-	-	-	-	-
	- Design , Consultancy & Supervison 6%	46,128,131.71	77,291.90	0.17	3,080,940.06	6.68
	- Risk of Quantity Variation @7%	53,816,153.66	90,173.88	0.17	3,594,430.06	6.68
	- Market Fluctuation @ 4.5%	34,596,098.78	57,968.92	0.17	2,310,705.04	6.68
	Sub Total EPC Turnkey Cost	138,384,395.12	231,875.70	0.17	9,242,820.17	6.68
	Security /Hard Area @ 4%	30,752,087.81	51,527.93	0.17	2,053,960.04	6.68
TOTAL PROJECT COST (SECTION-I)		937,938,678.06	1,571,601.94	0.17	62,645,781.13	6.68

3.4 QUARTERLY PROGRESS BY BILL OF QUANTITIES

ITEM NO	DESCRIPTION	UNIT	QUANTITY AS PER BOQ	QUARTER # 01		QUARTER # 02		CUMULATIVE	
				QUANTITY	% age	QUANTITY	% age	QUANTITY	% age
BILL NO.1 EARTH WORK									
101	Clearing & Grubbing	SM	29876.0	21000	70.29	28131.25	94.16	49131.3	164.45
104	Compaction of Natural Ground	SM	29876.0	18000	60.25	11888.38	39.79	29888.4	100.04
106a	Structure Excavation in Unsuitable Material	CM	3762.0	-	-	-	-	-	-
108a	Formation of Embankment From Road way Excavation in Common Material	CM	4000.0	-	-	6394.77	159.87	6,394.77	159.87
109a	Subgrade Preparation in Earth Cut	SM	4352.0	-	-	59194.50	1360.17	59,194.50	1360.17
BILL NO.2 SUB BASE & BASE COURSE									
201	Granular Sub Base	CM	45569.0	-	-	15460.08	33.93	15460.1	33.93
202	Agregate Base Course	CM	22868.0	-	-	-	-	-	-
BILL NO.3 SURFACE COURSES AND PAVEMENT									
NS	Grooving of existing asphalt layers at every 5M interval	SM	63,000.0	-	-	38872.50	61.70	38872.5	61.70
BILL NO.4b STRUCTURES (Culverts)									
NS	Widening and repair of existing Culverts at RD 1+290 & 5+692	No	2	-	-	-	-	-	-
NS	Construction of New Culverts No. of Span x Span Width x Height								
	1 x 2 x 1.5	No	7	-	-	3.24	46.36	3.24	46.36
	1 x 3 x 1.5	No	3	-	-	0.09	3.07	0.09	3.07
	2 x 3 x 1.5	No	2	-	-	1.33	66.59	1.33	66.59
	3 x 3 x 1.5	No	1	-	-	-	-	-	-
	5 x 3 x 1.5	No	1	-	-	0.19	18.74	0.19	18.74
BILL NO. 7 DIVERSION									
NS	Diversion for Traffic During Road Construction	KM	9	0.378	4.200	0.68	7.52	1.06	11.72

3.5 CULVERTS PHYSICAL PROGRESS STATUS



3.6 DETAILED INFORMATION OF LABORATORY TEST REPORTS

ITEM	DESCRIPTION OF MATERIAL	TEST ITEM	PREVIOUS QUARTER (1st)			THIS QUARTER (2nd)			TOTAL UP-TODATE			REMARK
			NO OF TESTS	PASSED	FAILED	NO OF TESTS	PASSED	FAILED	NO OF TESTS	PASSED	FAILED	
CONCRETE	Fine Aggregate	Sieve Analysis	0	0	0	3	3	0	3	3	0	
		Specific Gravity	0	0	0	2	2	0	2	2	0	
		Absorption	0	0	0	2	2	0	2	2	0	
		Unit Weight	0	0	0	2	2	0	2	2	0	
		Soundness	0	0	0	1	1	0	1	1	0	
		Sand Equivalent	0	0	0	1	1	0	1	1	0	
		Organic Impurities	0	0	0	1	1	0	1	1	0	
	Coarse Aggregate	Sieve Analysis	0	0	0	3	3	0	3	3	0	
		Specific Gravity	0	0	0	2	2	0	2	2	0	
		Absorption	0	0	0	2	2	0	2	2	0	
		Unit Weight	0	0	0	2	2	0	2	2	0	
		Soundness	0	0	0	1	1	0	1	1	0	
		Abrasion	0	0	0	1	1	0	1	1	0	
	COMPRESSIVE STRENGTH	LEAN CONCRETE	0	0	0	11	11	0	11	11	0	
		CLASS "B" CONCRETE	0	0	0	0	0	0	0	0	0	
		CLASS "A" CONCRETE	0	0	0	0	0	0	0	0	0	
	Cement	Specific Gravity	0	0	0	0	0	0	0	0	0	
		Setting Time	0	0	0	1	1	0	1	1	0	
		Compressive Strength	0	0	0	1	1	0	1	1	0	
	Water	Chemical Test	0	0	0	1	1	0	1	1	0	
	Steel Bar	Tensile Strength	0	0	0	1	1	0	1	1	0	
Elongation		0	0	0	1	1	0	1	1	0		
Bend		0	0	0	1	1	0	1	1	0		

DETAILED INFORMATION OF LABORATORY TEST REPORT

ITEM	DESCRIPTION OF MATERIAL	TEST ITEM	PREVIOUS QUARTER (1st)			THIS QUARTER (2nd)			TOTAL UP-TODATE			REMARK
			NO OF TESTS	PASSED	FAILED	NO OF TESTS	PASSED	FAILED	NO OF TESTS	PASSED	FAILED	
QUALITY TESTS OF EMBANKMENT/ PAVEMENT MATERIAL	Borrow Area	Sieve Analysis	9	9	0	5	5	0	14	14	0	
		Plasticity Index	5	5	0	5	5	0	10	10	0	
		Proctor Test	5	5	0	5	5	0	10	10	0	
		Abrasion	0	0	0	2	2	0	2	2	0	
		CBR Test	5	5	0	4	4	0	9	9	0	
	Embankment	Gradation	0	0	0	0	0	0	0	0	0	
		Plasticity Index	0	0	0	0	0	0	0	0	0	
		Moisture Density	0	0	0	0	0	0	0	0	0	
		CBR Test	0	0	0	0	0	0	0	0	0	
	NGC/Sub Grade	Gradation	0	0	0	11	11	0	11	11	0	
		Plasticity Index	0	0	0	10	10	0	10	10	0	
		Moisture Density	0	0	0	10	10	0	10	10	0	
		CBR Test	0	0	0	11	11	0	11	11	0	
	Sub Base	Gradation	0	0	0	6	6	0	6	6	0	
		Plasticity Index	0	0	0	5	5	0	5	5	0	
		Moisture Density	0	0	0	6	6	0	6	6	0	
		CBR Test	0	0	0	4	4	0	4	4	0	
		Abrasion	0	0	0	2	2	0	2	2	0	
		Sand Equivalent	0	0	0	2	2	0	2	2	0	
	Agg.Base Course	Gradation	0	0	0	5	5	0	5	5	0	
		Abrasion	0	0	0	2	2	0	2	2	0	
		Specific Gravity	0	0	0	1	1	0	1	1	0	
		Sand Equivalent	0	0	0	2	2	0	2	2	0	
		Soundness	0	0	0	1	1	0	1	1	0	
		Plasticity Index	0	0	0	3	3	0	3	3	0	
		Proctor	0	0	0	3	3	0	3	3	0	
	CBR Test	0	0	0	2	2	0	2	2	0		
	FIELD DENSITY TEST	NGC	31	30	1	25	22	3	56	52	4	
EMBANKMENT		3	2	1	14	14	0	17	16	1		
SUB GRADE		1	1	0	47	42	5	48	43	5		
SUB BASE		0	0	0	38	31	7	38	31	7		
AGG.BASE COURSE		0	0	0	0	0	0	0	0	0		

ENVIRONMENTAL COMPLIANCE MONITORING

4.1 Introduction:

Environmental Monitoring Compliance of each activity of the road component is being done according to the Environment Management and Monitoring Plan (EMMP) of the EDF/EIA report, duly approved by the USAID Mission Environment Officer (MEO).

Key roles and responsibilities of the Environmental Compliance Officer are as under:

- Environmental Monitoring Compliance of each activity during the construction phase, according to the Environment Management and Monitoring Plan (EMMP).
- Seek and ensure community involvement in environment related matters.
- Reporting of environmental non-compliance related issues and suggest remedial measures for improvement.
- Assist in implementing of EMMP.

1. Existing Environmental Conditions in the Area of Influence:

Land use of the project area includes residential and commercial areas of varying density including educational and health facilities, along the project road. The project area consists mostly of barren land strips and mountains. At the start of the project (about 09 kilometer), the land is plain and barren with sparse vegetation, while the rest of it consists of rocky mountainous terrain. An abandoned railway track runs along the road alignment till the end point of the project and crosses the alignment at different locations. There are several surface water bodies running across and along the project road such as the Wazir-Dand Canal, Surkamar River and Takhtabeg River. Ground water is available in the project area and used for both drinking and irrigation purposes. There are few vegetation strips and trees situated within the Right of Way (ROW) of project road near 21+500 KM. Cattles have been found grazing at some locations during the site visit but no wild animals have been noticed near the project road.

2. Potential Environmental Impacts of the Project Road:

Following are the identified potential impacts of the project as per Environment Review Report:

a) Potential Positive Impacts

- Accessibility to the Khyber Agency and Torkham Border from Khyber Pakhtunkhwa province and will improve the road linkage b/w Pakistan and Afghanistan.
- An improved trade corridor b/w Pakistan and Afghanistan.
- Better conditions for law enforcement and improved security control in the border areas.
- Generate economic and social opportunities for the local residents.

b) Potential Negative Impacts

Project does not have potential significant adverse impacts, however, during strengthening and improvement of the road, the following potential impacts are anticipated which could be avoided, localized or mitigated by adopting the proper mitigation measures:

- Noise and air pollution.
- Traffic congestion at diversions.
- Health and safety issues of public and workers.
- Waste generation.
- Disturbance to people.
- Soil erosion and contamination.
- Potential impact of blasting if required at quarry area.
- Surface and ground water pollution.

4.2 Environment Compliance

1. Procedure:

To comply with the Environment, Health, Safety and Social protocols, a comprehensive Proforma has been prepared. Initially the progress of work was limited; therefore fortnight visits have been conducted during the reporting period. Documentation of the site visit is being carried out subsequently after each visit.

2. General Condition of the Section – 1 (0+000 to 9+000 KM):

During the reporting period, the work is being executed by the contractor (FWO) on section 1 (0+000 to 9+000 KM) of the Project. The existing road condition varies from poor to fair. Nearly 4 KM of section-I passes through commercial area while rest of the road passes through plain terrain. Warsak Lift Canal and many Non-perennial streams especially the Khyber Khwar crosses the road. Existing culverts have lost their hydraulic/structural capacity and thus either requires reconstruction or rehabilitation.

4.3 Progress during the Quarter # 02 (Jan – March 2013)

During the reporting period six site visits have been made on fortnight basis. The site visit reports have already been submitted to the concerned USAID officials. Out of total six environmental monitoring visits, environmental compliance has been observed satisfactory in two visits as per EMMP of the project. However in four reports, the compliance was observed not in accordance with Environment, Health, Safety and Social protocols, described in the EMMP. The Contractor's camps and Machinery are maintained in good conditions. The issues noticed during the reporting period are summarized below:

S. NO	MONTH	ENVIRONMENTAL ISSUES
1	January 2013	<ul style="list-style-type: none"> • Damages to PTCL Telephone cables at KM 5+560. • Dumping of excavated material in graveyard, • Drainage problems at culvert's construction sites and quarry areas. • No road's traffic signs and speed checking for the safety of locals. • Non availability of Environment Specialist/ Expert on site from FWO/NESPAK side
2	February 2013	<ul style="list-style-type: none"> • No sprinkling of water on road's diversion and near the residential areas. • Non availability of Environment Specialist/Expert on site from FWO/NESPAK side. • No Health and Safety arrangement at working sites. • Damages to public infrastructure. • Land leveling and refilling of adjacent previous quarry sites for sustainable use. • No records of EHS (Environment, Health and Safety) • Non availability of personal protective equipment
3	March 2013	<ul style="list-style-type: none"> • Damages to graves in quarry area. • No sprinkling of water on road's diversion and near the residential areas. • Drainage problems at culvert's construction sites and quarry areas. • Non availability of Environment Specialist/ Expert on site from FWO / NESPAK side. • No Health and Safety arrangement at working sites. • No road's traffic signs and speed checking for the safety of people. • No records of EHS (Environment, Health and Safety) • People demanded for constructions of access to utilize the under construction culverts as underpass. • Non availability of personal protective equipment

SECURITY REPORT

According to USAID's threat assessment, the risk level in KP & FATA is 'HIGH'. The implementing partners (IPs) operating in KP / FATA are therefore advised to exercise heightened security awareness all the times. The security related activities undertaken during last quarter are summarized as below:

- January 29, 2013: A Security meeting with Project Manager (CMEP) was held to discuss general security situation pertaining to the project.
- January 30, 2013: A Security Plan along with General orders was circulated to all staff members on the direction of Project Manager (CMEP).
- Feb 14, 2013: Quality Assurance Manager (Road) and USAID Representative, Mr. Jalail ur Rahman along with Security Officer visited Peshawar- Torkham road project followed by a meeting with representative of FWO and NESPAK in their office. During the visit following security related matters were discussed:
 1. Security Officer suggested to USAID representative that since Political Administration and Frontier Corps unit (Khyber Rifles) are responsible for the security and protection of all the areas along with the project in Khyber Agency, there is a need to bring the official status of AGES Consultants in their notice, so that they recognize and help us in case of any emergency. More over the Security Officer will be able to carry out liaison visit to the concerned authorities i.e. Political Administration and Frontier Corps unit (Khyber Rifles) after they get informed about our role in the road project.
 2. Security Officer suggested to USAID representative that although we are maintaining low profile as a security measure but there is still a need of possession of personal weapon by the Security Officer, which he can use for guarding against any attack and for personal protection of our staff and visitors.
- Feb 19, 2013: The Security Officer (CMEP) along with security Advisor (M&E Project) attended USAID monthly safety & security meeting at Serena hotel Islamabad. During this meeting discussion was made about security of the USAID Projects.
- March 7, 2013: Vehicle safety plan received from USAID was sent to Project Manager (CMEP) and Quality Assurance Manager (Road).
- March 25, 2013: A Security meeting was carried out, with Project Manager (CMEP) and Quality Assurance Manager (Road) along with others. During the meeting, general security situation was discussed.
- March 25, 2013: USAID threat assessment was shared with the Project Manager (CMEP).

Reportedly, there have been five security related incidents occurred during the quarter, at different locations along the project site; three of them involved attacks on NATO Containers while two included Bomb blasts in Jamrud bazar. In total, 19 persons were killed and 78 injured. Keeping in view the fragile security situation in the project area (Khyber Agency) and the personal security of the project staff, it is recommended that a joint meeting with FATA Secretariat officials, FWO and Political Administration should be arranged to discuss the security of M&E Consultants' staff and provide them with the necessary emergency contacts. In addition, all employees should be encouraged to accept personal responsibility of their own safety and of their subordinates by adhering to the following:

- Vary routes and timings to and from work.
- Carry cell phone all the times for information of situation.
- Check interior and exterior of their vehicles prior to getting into it (for any suspicious item).
- Keep the doors locked and windows closed when traveling in vehicles.
- Maintain a low personal profile by not doing anything that draw attention to their self.
- Must be alert to the situation around them.
- The colleagues must share and be aware of each other's daily plan- so in case of emergency they can be contacted conveniently.
- In traffic jams, always try to leave space for maneuvering-always leave their self on exit.
- Be prepared to take evasive action.
- Avoid choke points in travel.
- If being harassed or followed try contact police/khasadar force/ Frontier Corps personnel, never lead the person back to home or office.
- Never give out your personal information as Project name, project sponsor, family members, addresses and telephone numbers in an open sitting.
- Follow security orders and instructions.

APPENDICES

6.1 ELECTRONIC COMMUNICATION/CORRESPONDENCE WITH USAID

S. No	Date	Subject	Addressed	
			From	To
1	14-Jan-13	Field Visit Report	M & E CONSULTANTS	USAID
2	18-Jan-13	Document Required from FWO	USAID	NESPAK
3	22-Jan-13	Draft Construction Schedule & QAP by FWO	USAID	M & E CONSULTANTS
4	23-Jan-13	Project Implementation Letter	USAID	M & E CONSULTANTS
5	30-Jan-13	Environmental Compliance Report-Jan 2013	M & E CONSULTANTS	USAID
6	03-Feb-13	Picture for Jan 26-31, 2013	M & E CONSULTANTS	USAID
7	08-Feb-13	Progress Report for the month of Jan, 2013	M & E CONSULTANTS	USAID
8	25-Feb-13	Environmental Compliance Report	M & E CONSULTANTS	USAID
9	06-Mar-13	Progress Report for the month of Feb, 2013	M & E CONSULTANTS	USAID
10	11-Mar-13	Issues/Impediments-Feb, 2013	M & E CONSULTANTS	USAID
11	14-Mar-13	Implementation of NHA CSR 2011	USAID	M & E CONSULTANTS
12	15-Mar-13	Quarterly Progress Report of Peshawar-Torkham Road (Dead Line)	USAID	M & E CONSULTANTS
13	20-Mar-13	Comparison of Rates of NHA CSR 2009 with 71% premium and NHA CSR 2011	M & E CONSULTANTS	USAID
14	25-Mar-13	Report on ongoing activities with observation & recommendations-March 2013	M & E CONSULTANTS	USAID
15	25-Mar-13	Environmental Compliance Report-March 18th 2013	M & E CONSULTANTS	USAID

6.2 RECORD OF COORDINATION MEETING

Date	Meeting	Participants
10-Jan-13	Section-I; Review Meeting	USAID-AGES
14-Feb-13	Section-I; Review Meeting	USAID-AGES
27-Mar-13	Peshawar-Torkham Road Project Review Meeting	USAID, FATA, FWO, NESPAK, AGES

PROJECT PHOTOGRAPHS

PAVEMENT STRUCTURE



Lat 33; 59; 53.1, Lon 71; 24; 16.9
KM: 1 + 750 To 1 + 825
Common Excavation



Lat 33; 59; 55.3, Lon 71; 24; 20
KM: 1 + 900 to 1 + 950 (LHS Shoulder)
Common Excavation



Lat 34; 0; 13.7, Lon 71; 22; 39
KM: 4 + 625 To 4 + 700
Common excavation



Lat 34; 0; 6.5, Lon 71; 22; 59.5
KM: 4 + 350 To 4 + 450
Road cutting



Lat 34; 0; 5.6, Lon 71; 23; 50.5
KM: 2 + 550 To 2 + 650 (RHS Shoulder)
NGC preparation



Lat 33; 59; 58.9, Lon 71; 23; 33.2
KM: 3 + 125 To 3 + 275
Sub grade preparation



Lat 34; 0; 20.9, Lon 71; 20; 12.6
KM: 8 + 800 To 8 + 900
Sub grade layer is ready



Lat 34; 0; 9.3, Lon 71; 21; 56.9
KM: 5 + 925 To 5 + 975
Sub grade preparation



Lat 34; 0; 16, Lon 71; 20; 54.1
KM: 7 + 225 To 7 + 350
Dumping of material for sub base layer



Lat 34; 0; 5.3, Lon 71; 23; 50
KM: 2 + 625 To 2 + 675
Sub base preparation



Lat 34; 0; 9.2, Lon 71; 21; 57.1
KM: 5 + 600 To 5 + 825
Full width Sub base compaction



Lat 33; 59; 58.5, Lon 71; 23; 33.3
KM: 3 + 125 To 3 + 175
full width sub base 1st layer compaction



Lat 34; 0; 8.2, Lon 71; 21; 43
KM: 6 + 075 To 6 + 175
Sub base 1st layer preparation



Lat 34; 0; 14, Lon 71; 19; 55.2
KM: 8 + 825 To 8 + 950
Half width sub base compaction



Lat 34; 0; 8, Lon 71; 22; 53.1
KM: 4+230
Placing of RCC Pipe service duct



Lat 34; 0; 7.6, Lon 71; 22; 55.1
KM: 4+180
laying of RCC Pipe for service duct



Lat 34; 0; 2, Lon 71; 19; 24.1
KM 9+600 LHS
Crush Plant



Lat 34; 0; 15.6, Lon 71; 20; 58.1
KM: 7+575 To 7+725
Dumping for Aggregate base course



Lat 34; 0; 16.8, Lon 71; 20; 46.4
KM: 7 + 450 To 7 + 575
Aggregate base course compaction



Lat 34; 0; 16.6, Lon 71; 20; 47.2
KM: 7+450 To 7+575
Aggregate base course compaction

CULVERTS

JANUARY/FEBRUARY 2013

MARCH 2013



**Lat 34; 0; 5.5, Lon 71; 23; 50.5
 KM: 2+611**



**Lat 34; 0; 5.5, Lon 71; 23; 50.5
 KM: 2+611**



**Lat 34; 0; 0, Lon 71; 23; 33.8
 KM: 3+081**



**Lat 34; 0; 0, Lon 71; 23; 33.8
 KM: 3+081**



**Lat 34; 0; 8.6, Lon 71; 22; 43.5
 KM: 4+480**



**Lat 34; 0; 8.6, Lon 71; 22; 43.5
 KM: 4+480**

JANUARY/FEBRUARY 2013

MARCH 2013



**Lat 34; 0; 6.8, Lon 71; 22; 16.6
 KM: 5+202**



**Lat 34; 0; 6.8, Lon 71; 22; 16.6
 KM: 5+202**



**Lat 34; 0; 7.9, Lon 71; 21; 49.2
 KM: 5 + 905**



**Lat 34; 0; 7.9, Lon 71; 21; 49.2
 KM: 5 + 905**



**Lat 34; 0; 8.0, Lon 71; 21; 44.0
 KM: 6 + 050**



**Lat 34; 0; 8.0, Lon 71; 21; 44.0
 KM: 6 + 050**

JANUARY/FEBRUARY 2013

MARCH 2013



**Lat 34; 0; 8.723, Lon 71; 21; 27.733
 KM: 6 + 191**



**Lat 34; 0; 8.723, Lon 71; 21; 27.733
 KM: 6 + 191**



**Lat 34; 0; 11.4, Lon 71; 21; 27
 KM: 6+501**



**Lat 34; 0; 11.4, Lon 71; 21; 27
 KM: 6+501**



**Lat 34; 0; 12.1, Lon 71; 21; 21.1
 KM: 6+648**



**Lat 34; 0; 12.1, Lon 71; 21; 21.1
 KM: 6+648**

JANUARY/FEBRUARY 2013

MARCH 2013



Lat 34; 0; 13.295, Lon 71; 21; 11.898
KM: 6 + 883



Lat 34; 0; 13.295, Lon 71; 21; 11.898
KM: 6 + 883



Lat 34; 0; 15.455, Lon 71; 20; 53.345
KM: 7 + 384



Lat 34; 0; 15.455, Lon 71; 20; 53.345
KM: 7 + 384

FIELD TESTING



Equipments in M&E Consultants lab



Equipments in M&E Consultants lab



Equipments in M&E Consultants lab



Equipments in M&E Consultants lab



Testing of sub base material in M&E Consultants Lab



Samples for Abrasion test in M&E Consultants Lab



Crushing of concrete cylinder in FWO Lab



FDT Test of base course trial section



Proctor test is in progress in FWO Lab



Proctor test is in progress in FWO Lab



Sample collection from base course by NESPAK & M&E staff



FDT Test is in progress

PHOTOGRAPHS OF ENVIRONMENTAL MONITORING



Damaged PTCL cable at KM 5+905



**Stagnant pool of water
at Jamrud Bazar KM 4+600**



**Dumping of excavated material on roadside at
KM 3+200**



**Excavated Material dumped in graveyard near
KM 5+630**



**Damage to public infrastructure due to
construction of culvert in Jamrud Bazar**



**During excavation process in the
quarry area (near KM 6+050)
some graves were exposed**